



16591

22 April 2025

PUBLIC NOTICE D01-214-2025

All interested parties are herein notified that the Commander, First Coast Guard District, has received revised application materials dated 11 February 2025 from Suffolk County Department of Public Works (SCDPW) for approval of location and plans for the replacement of a highway bridge over a navigable waterway of the United States.

BRIDGE, WATERWAY AND LOCATION: Smith Point Bridge across Narrow Bay (where it meets the Long Island Intercoastal Waterway), mile 6.1, on William Floyd Parkway, County Route 46, between the hamlets of Mastic Beach, Shirley and Smith Point County Park, Fire Island, Suffolk County, Town of Brookhaven, New York.

CHARACTER OF WORK: Proposed replacement of the double-leaf bascule-span bridge and approaches, to address major structural and operational deficiencies and other non-standard features, with a fixed high-level bridge constructed along an adjacent alignment to the west of the existing bridge.

The proposed bridge will have 11-foot-wide travel lanes, one each in the northbound and southbound direction, two 4-foot-wide shoulders, and one 12-foot-wide protected shared-use path for pedestrians and bicyclists. The proposed bridge will consist of eleven 165-foot-long prestressed concrete girder-spans supported on ten hammerhead piers and two-abutments with precast, prestressed concrete pile foundations. The proposed approaches will consist of a combination of fill type retaining wall structures and constructed roadway embankments.

Construction of the proposed bridge and approaches will be performed in two main stages with an approximate 32-month duration. Traffic will be maintained on the existing bridge while the replacement bridge is constructed on an adjacent alignment to the west. During the second stage, the traffic will be shifted from the existing bridge onto the proposed bridge to allow demolition of the existing bridge, anticipated to take approximately 8 months.

The existing bridge superstructure for the bascule span, two flanking spans, sixteen approach spans, pile bent pier caps supporting approach spans, three column piers supporting the flanking spans, and the abutments will be removed in their entirety. The existing timber piles supporting the bascule and flanking piers within the navigable channel will be removed to a minimum depth of three feet below the bottom of the channel. The existing concrete piles supporting the abutments on land will be removed to a minimum of one foot below proposed grade. The existing submarine cables powering the movable span will be de-energized, cut, capped, and abandoned in place. The existing bridge operation regulation in 33 CFR Section 117.799 will be removed upon issuance of the bridge permit and shifting of traffic to the new bridge. A local notice of mariners will be published on the U.S. Coast Guard Navigation Center website when the operation regulation is removed.

MINIMUM NAVIGATIONAL CLEARANCES:

The proposed replacement bridge will have clearances as described in the table below. The vertical clearance decreases from the current unlimited clearance when the existing bridge is in the open position. Vertical clearance is from low member elevation to Mean High Water (MHW). The proposed bridge spans across the entire navigable channel with pier foundations located outside the limits of the navigable channel. There is an additional 15 feet of horizontal clearance on either side of the limits the 100-foot navigable channel to the bridge piers.

	Vertical (above MHW)	Horizontal
Existing (Movable)	Closed 16 feet Open Unlimited	55.5 feet normal to the centerline of the navigable channel between faces of fender system.
Proposed (Fixed)	55.6 feet	130 feet normal to the centerline of the navigable channel. USACE maintains a 100-foot navigable channel.

Datum: NAVD88

ENVIRONMENTAL CONSIDERATIONS:

The Federal Highways Administration (FHWA) is the lead Federal agency for satisfying the requirements of the National Environmental Policy Act (NEPA). FHWA is acting on behalf of the U.S. Coast Guard for all environmental control laws. A Final Design Report/Environmental Assessment (DR/EA) was completed in March 2019 and a Finding of No Significant Impact (FONSI) was issued on 1 July 2019. FWHA supplemented the FONSI on 26 November 2024 with a re-evaluation for the Modified Preferred Alternative, concluding that the 2019 Final DR/EA remained valid. The U.S. Coast Guard has tentatively determined that the proposed action will not have a significant impact for the purposes of NEPA for the project. Documents are available for review by emailing a request to public.works@suffolkcountyny.gov.

A Water Quality Certification (WQC) in accordance with Section 401 of the Clean Water Act, as amended, for this project was applied for on 20 August 2021. Resubmission via a joint application to the U.S. Army Corps of Engineers (USACE) and the New York State Department of Environmental Conservation (NYSDEC) with additional requested information and updates is pending. 0.12 acres of tidal wetlands are anticipated to be permanently impacted. 0.69 acres of tidal wetlands are anticipated to be permanently shaded. 0.33 acres of tidal wetlands are anticipated to be temporarily impacted. 0.66 acres of tidal wetlands are anticipated to be temporarily shaded. An on-site wetland mitigation strategy will be implemented to restore the wetlands that will be both permanently and temporarily impacted by the proposed project.

New York State Department of State (NYSDOS) provided general concurrence that the project meets general consistency concurrence criteria with no objection to FHWA funding. SCDPW has made a preliminary determination that the project will be consistent with the NYSDOS Coastal Management Program and NYSDOS is anticipated to concur.

The bridge is located in the floodplain. The 100-year flood elevation is +8.0 feet. The elevation of the low member of the navigation span is 55.6 feet MHW. Elevations are referenced to NAVD88 datum. See plan sheet 2 (of 4) for the approximate cubic yards of fill material that will be placed below mean high water level for the construction of the bridge and approaches. The cubic yards for each footing vary.

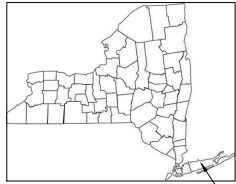
SOLICITATION OF COMMENTS:

Mariners are requested to comment on the proposed navigation clearances, placement of a bridge protective system and other navigational safety issues, including need for clearance gauges and extent of nighttime navigation to determine the need for bridge lighting. Interested parties are requested to express their views, in writing, on the proposed bridge project including its possible impacts to navigation.

We will forward comments of an environmental nature such as those regarding wildlife refuges, public parks, historic sites, wetlands, floodplain issues, air, water quality etc. to the Federal Highway Administration. Comments will be received for the record at the address noted in the header or via email: SMB-D1Boston-Bridges-PublicNotices@uscg.mil through May 23, 2025.

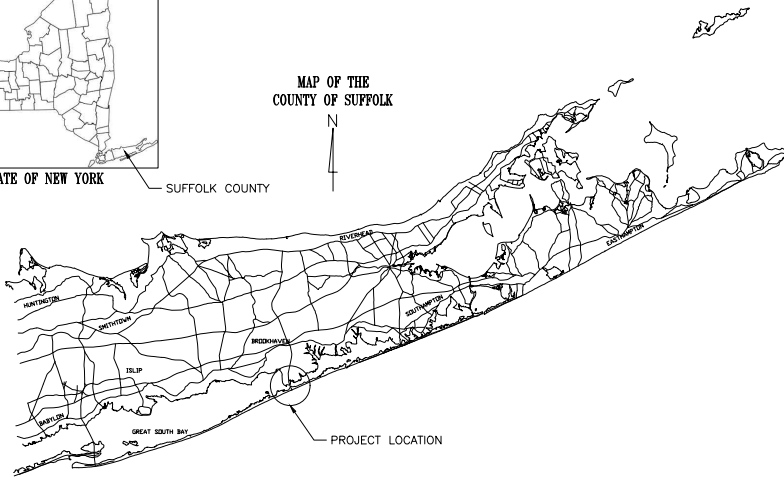
Map of location and plans attached.

G. P. Hitchen
Bridge Program Manager
U. S. Coast Guard
By direction



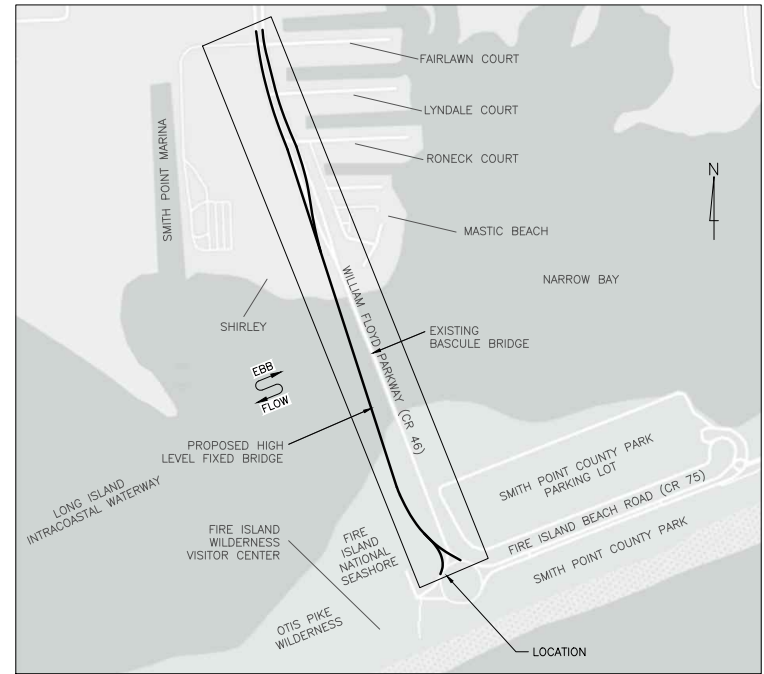
MAP OF THE COUNTY OF SUFFOLK

STATE OF NEW YORK
SUFFOLK COUNTY

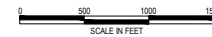


THIS PROJECT IS LOCATED BETWEEN THE SOUTHERN END OF MASTIC BEACH/SHIRLEY AND SMITH POINT COUNTY PARK/FIRE ISLAND; ALL IN THE TOWN OF BROOKHAVEN, NEW YORK.

VICINITY MAP



LOCATION MAP



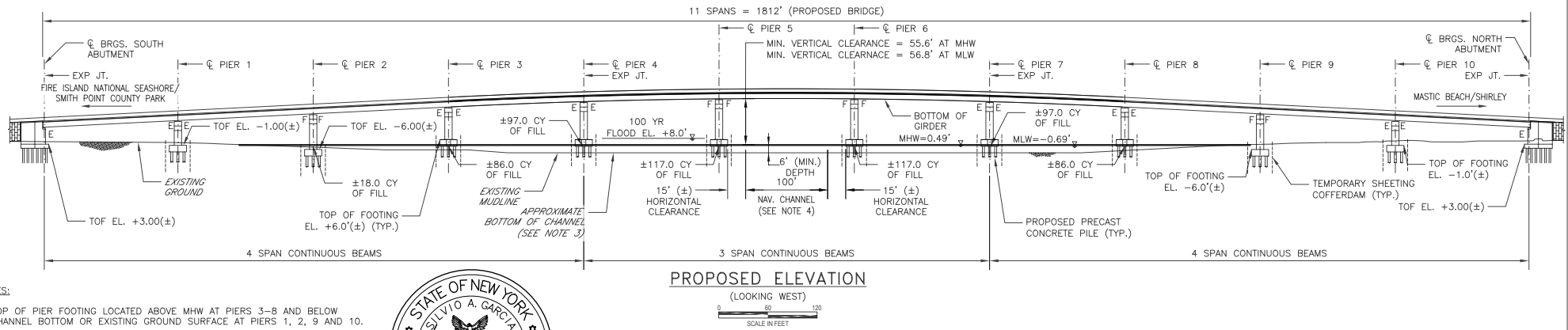
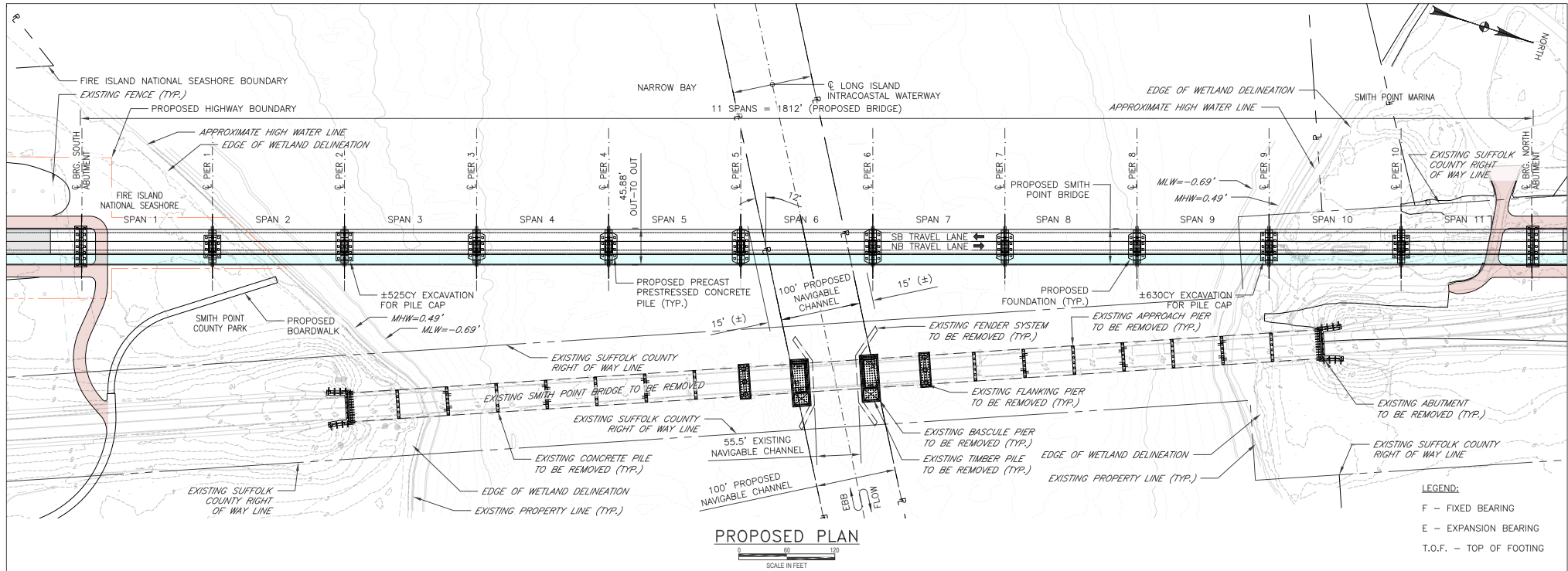
NOTES:

1. THE PROPOSED PROJECT DOES NOT INVOLVE WORK IN, OR ADJACENT TO, A WILDLIFE OR WATERFOWL REFUGE.
2. CORRESPONDENCE WITH SHPO INDICATES THAT IT IS NOT ANTICIPATED THAT CONSTRUCTION OF THE PROPOSED PROJECT WILL OCCUR WITHIN PREVIOUSLY UNDISTURBED AREAS THAT HAVE THE POTENTIAL TO CONTAIN ARCHEOLOGICAL RESOURCES.
3. CORRESPONDENCE WITH SHPO HAS CONFIRMED THAT THE SMITH POINT BRIDGE IS ELIGIBLE FOR LISTING ON THE STATE AND NATIONAL REGISTERS OF HISTORIC PLACES. IT WAS DETERMINED THAT THERE IS NO PRUDENT AND FEASIBLE ALTERNATIVE TO DEMOLITION OF THE EXISTING BRIDGE.
4. CONCEPTUAL PLANS UTILIZED TO OBTAIN COAST GUARD BRIDGE PERMIT.



03/31/2025

LOCATION	BETWEEN THE HAMLETS OF MASTIC BEACH & SHIRLEY AND SMITH POINT COUNTY PARK/FIRE ISLAND TOWN OF BROOKHAVEN SUFFOLK COUNTY, NEW YORK	REPLACEMENT OF SMITH POINT BRIDGE WILLIAM FLOYD PARKWAY (CR 46) OVER NARROW BAY, MILE POINT 6.1	SCALE: AS SHOWN
APPLICATION BY	SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS	LOCATION AND VICINITY MAPS	DATE 02/18/25
CONSULTANT	HARDESTY & HANOVER, LLC		PROJECT NO. CP5813 SHEET NO. 1 OF 4

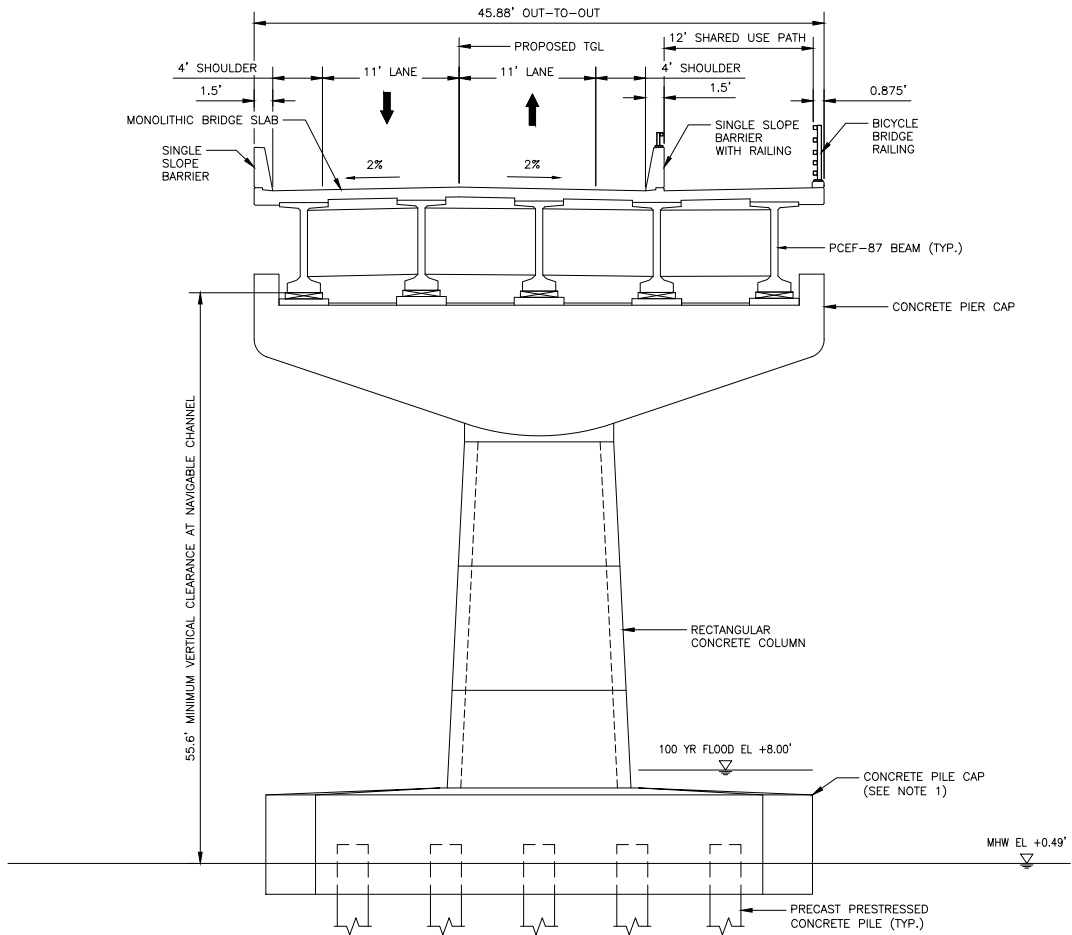


- NOTES:**
1. TOP OF PIER FOOTING LOCATED ABOVE MHW AT PIERS 3-8 AND BELOW CHANNEL BOTTOM OR EXISTING GROUND SURFACE AT PIERS 1, 2, 9 AND 10.
 2. ALL ELEVATIONS ARE IN THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
 3. THE BOTTOM OF CHANNEL ELEVATION AT THE CENTERLINE OF CHANNEL IS APPROXIMATELY -9.51 FEET AT SMITH POINT BRIDGE.
 4. 100' MEASURED NORMAL TO NAVIGABLE CHANNEL.
 5. CONCEPTUAL PLANS UTILIZED TO OBTAIN COAST GUARD BRIDGE PERMIT.



03/31/2025

LOCATION	BETWEEN THE HAMLETS OF MASTIC BEACH & SHIRLEY AND SMITH POINT COUNTY PARK/FIRE ISLAND TOWN OF BROOKHAVEN SUFFOLK COUNTY, NEW YORK	REPLACEMENT OF SMITH POINT BRIDGE WILLIAM PLOYD PARKWAY (CR 46) OVER NARROW BAY, MILE POINT 6.1	SCALE: AS SHOWN
APPLICATION BY	SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS	PROPOSED BRIDGE PLAN AND ELEVATION VIEWS	DATE 03/31/25 PROJECT NO. CP5813 SHEET NO. 2 OF 4
CONSULTANT	HARDESTY & HANOVER, LLC		



PROPOSED BRIDGE SECTION

SECTION IS LOOKING NORTH



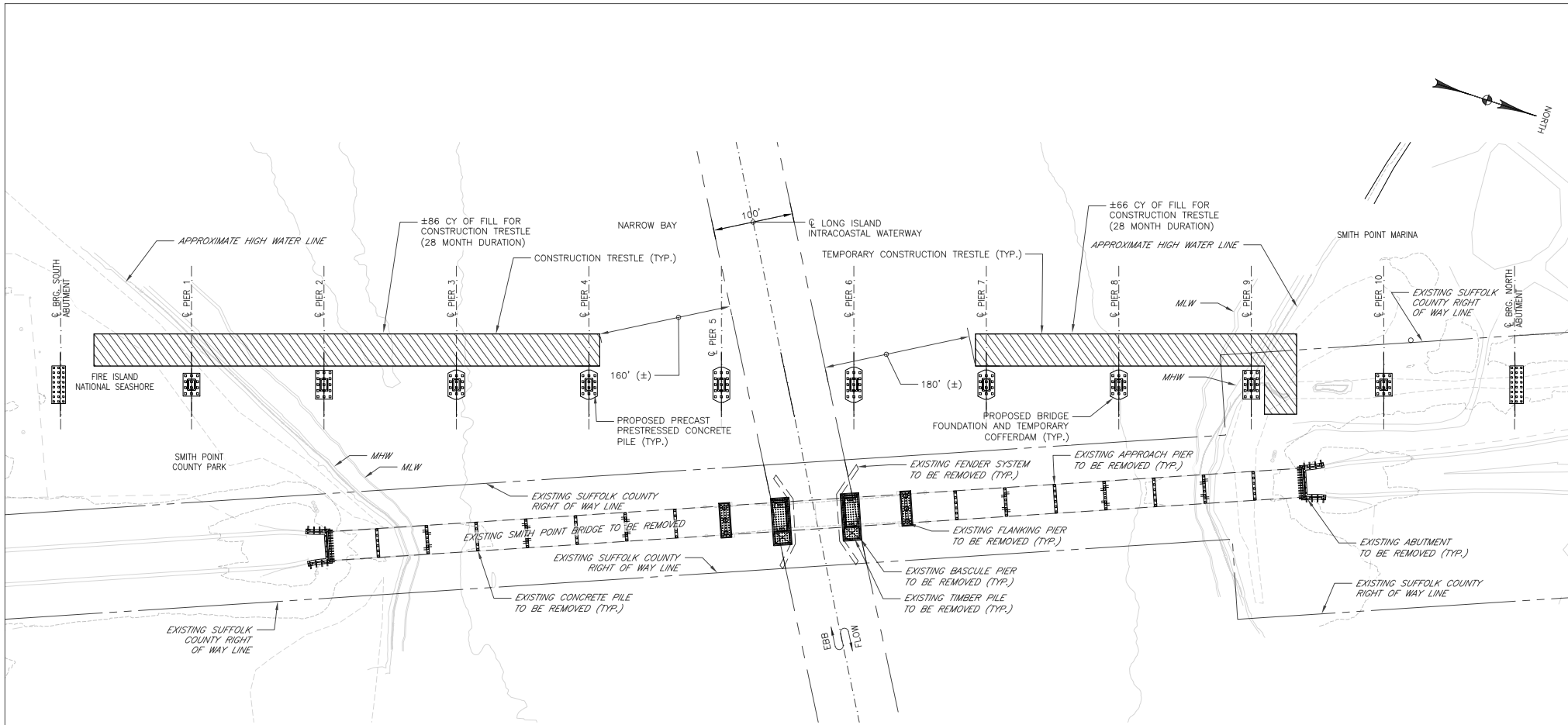
NOTES:

1. TOP OF PIER FOOTING LOCATED ABOVE MHW AT PIERS 3-8 AND BELOW CHANNEL BOTTOM OR EXISTING GROUND SURFACE AT PIERS 1, 2, 9 AND 10.
2. ALL ELEVATIONS ARE IN THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
3. CONCEPTUAL PLANS UTILIZED TO OBTAIN COAST GUARD BRIDGE PERMIT.



03/31/2025

LOCATION	BETWEEN THE HAMLETS OF MASTIC BEACH & SHIRLEY AND SMITH POINT COUNTY PARK/FIRE ISLAND TOWN OF BROOKHAVEN SUFFOLK COUNTY, NEW YORK	REPLACEMENT OF SMITH POINT BRIDGE WILLIAM FLOYD PARKWAY (CR 46) OVER NARROW BAY, MILE POINT 6.1	SCALE: AS SHOWN
APPLICATION BY	SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS	PROPOSED BRIDGE TYPICAL SECTION VIEWS	DATE 02/18/25
CONSULTANT	HARDESTY & HANOVER, LLC		PROJECT NO. CFS813 SHEET NO. 3 OF 4



PLAN VIEW
SCALE IN FEET

LEGEND:
 CONSTRUCTION TRESTLE

- NOTES:
1. ALL ELEVATIONS ARE IN THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
 2. BOTTOM OF TEMPORARY CONSTRUCTION TRESTLE ELEVATION TO BE INSTALLED ABOVE THE 50 YEAR FLOOD ELEVATION OF +4.80', WHICH IS 4.31' ABOVE THE MEAN HIGH WATER ELEVATION OF +0.49'.
 3. CONCEPTUAL PLANS UTILIZED TO OBTAIN COAST GUARD BRIDGE PERMIT.



03/31/2025

LOCATION	BETWEEN THE HAMLETS OF MASTIC BEACH & SHIRLEY AND SMITH POINT COUNTY PARK/FIRE ISLAND TOWN OF BROOKHAVEN SUFFOLK COUNTY, NEW YORK	REPLACEMENT OF SMITH POINT BRIDGE WILLIAM FLOYD PARKWAY (CR 46) OVER NARROW BAY, MILE POINT 6.1	SCALE: AS SHOWN
APPLICATION BY	SUFFOLK COUNTY DEPARTMENT OF PUBLIC WORKS	TEMPORARY STRUCTURE PLAN	DATE 02/18/25
CONSULTANT	HARDESTY & HANOVER, LLC		PROJECT NO. CFS813 SHEET NO. 4 OF 4